

DOOR SCHEDULE

DOOR No.	HEIGHT	WIDTH	TYPE	NOTES	GLAZING
D.01	2100	2150	SLIDING DOOR	ALUMINIUM	YES
D.02	2100	820	SWING	TIMBER	NO
D.03	2100	820	SWING	TIMBER	NO
D.04	2100	820	SWING	ALUMINIUM	NO
D.05	2100	820	SWING	TIMBER	NO
D.06	2100	820	SWING	TIMBER	NO

WINDOW SCHEDULE

WINDOW No.	HEIGHT	WIDTH	SILL HEIGHT	NOTES
W.01	1800	2150	300	FIXED
W.02	900	1200	1200	AWNING
W.03	900	1200	1200	AWNING
W.04	1275	870	N/A	SKYLIGHT
W.05	1275	870	N/A	SKYLIGHT
W.06	1275	870	N/A	SKYLIGHT



Floor Plan

1:50

REV.

DRAWING No.

PROJECT No.

DRAWN: T.D.GUNN
AUTH: T.D.GUNN
STATUS: CONVERSION

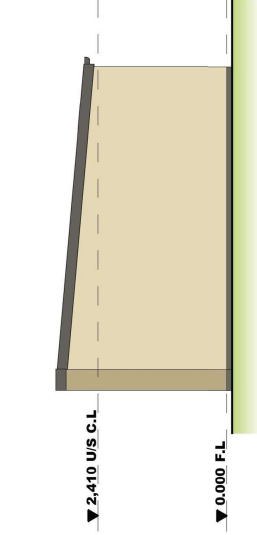
DATE: 28/04/2021
DATE: 28/04/2021
SCALE: 1:50

003

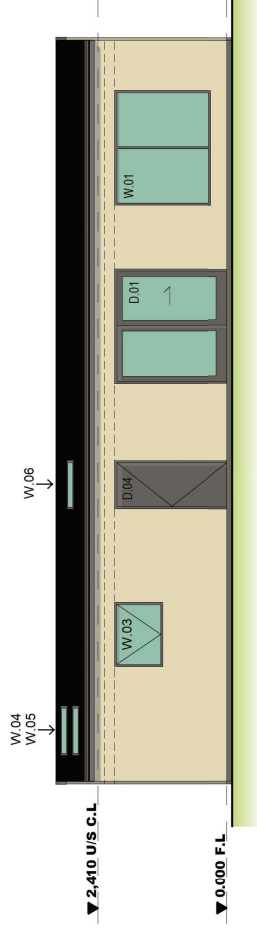
TG 001

PROPOSED VISITOR ACCOMMODATION AT 28 OLIVE STREET, NEWSTEAD

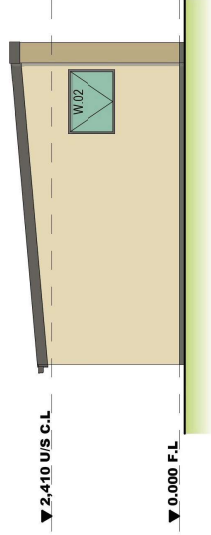
Floor Plan



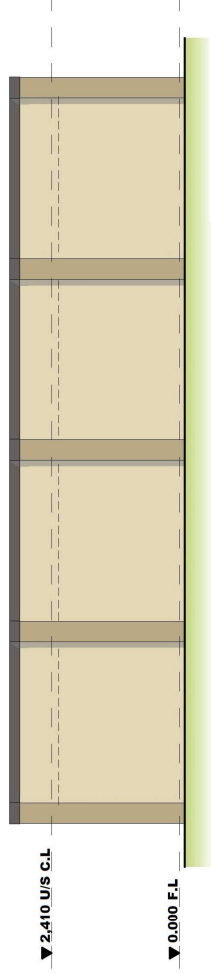
Southern Elevation



Eastern Elevation

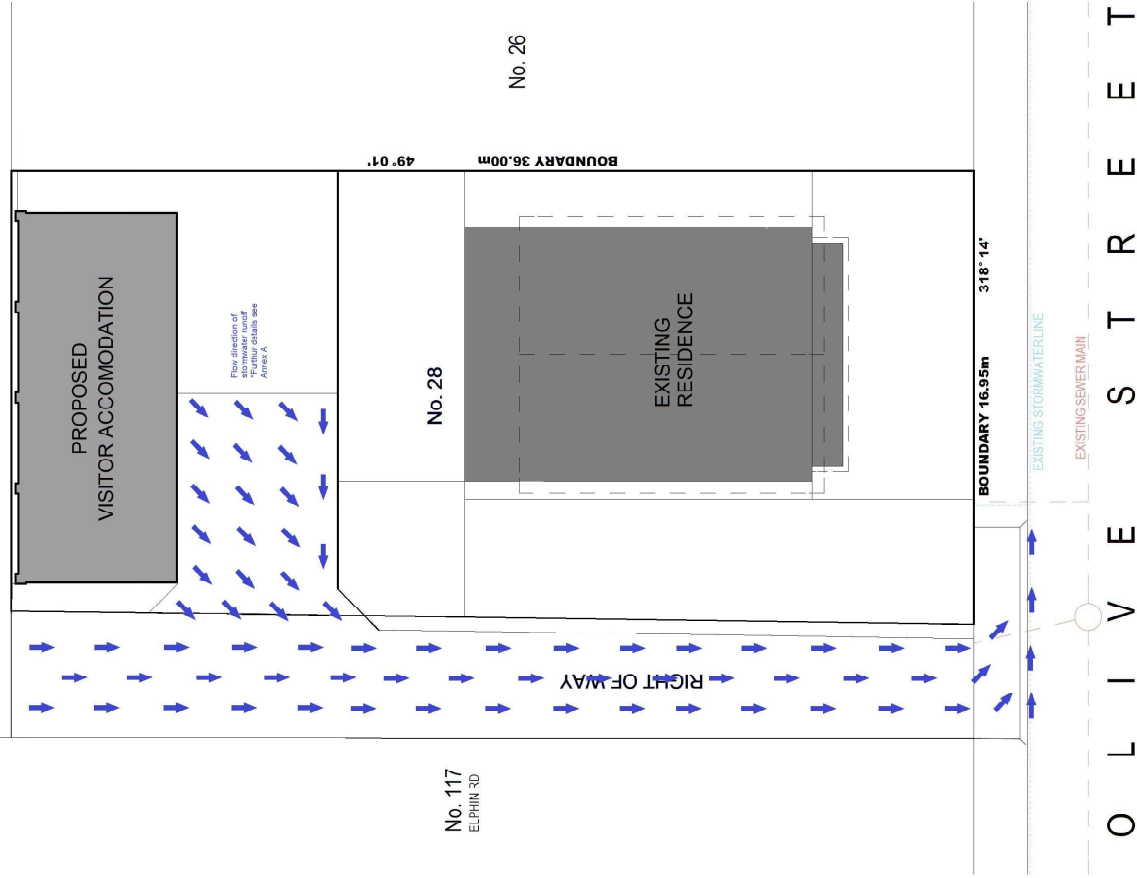


Northern Elevation



Western Elevation

		DRAWN: T.D.GUNN	DATE: 28/04/2021	PROJECT No. TG 001	DRAWING No. 001	REV. 2	PROPOSED VISITOR ACCOMMODATION AT 28 OLIVE STREET, NEWSTEAD FOR T.D.GUNN Elevations
		AUTH: T.D.GUNN	DATE: 28/04/2021				
		STATUS: CONVERSION					
		SCALE: 1 : 100					



		DRAWN: T.D.GUNN		DATE: 15/06/2021	PROJECT No.	DRAWING No.	REV.
		ALTH: T.D.GUNN		DATE: 15/06/2021	TG 001	004	1
		STATUS: CONVERSION		SCALE: 1 : 200			
PROPOSED VISITOR ACCOMMODATION AT 28 OLIVE STREET, NEWSTEAD FOR T.D.GUNN Stormwater Runoff							

Proposed Surfaces



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Version: 2, Version Date: 24/06/2021

ANNEX A to DA0251/2021

Parking Space Surface Treatment.

"E6.0 Parking and Sustainable Transport Code E6.6.1 A1 (b)" states "All parking, access ways, manoeuvring and circulation spaces must be formed and paved"

The finish to all these areas will be sealed asphalt as shown in submitted "Drawing 005 Rev1"

Earthworks required to achieve this will be the removal of 400mm of top soil in those areas where there is currently grass and replaced with 400mm of compacted road base prior to installation of asphalt surface. The existing areas marked as gravel in these location already consists of 400mm compacted road base.

Parking space access.

"E6.0 Parking and Sustainable Transport Code Table E6.3" Calls for a combined access and manoeuvring width of only 6.4m to access a 5.4m x 2.6m, this proposal has over 7.5m

A higher standard exists in *"AS/NZS 2890.1 Off Street Parking Table 1.1"* which provides 6 different classifications of off street parking. As the property is located in the zoning for General Residential and is primarily a domestic property the most accurate class identification is Class 1A described as *"Residential, domestic and employee parking"* the requirement under this table is *"three-point turn entry and exit into 90deg spaces only"* and subsequently under *"AS/NZS 2890.1 Off Street Parking Figure 2.2"* require an aisle width or only 5.8m. Therefore there is no requirement for a B85 single manoeuvre 5.4m radius in using this class.

However there is adequate manoeuvring clearance to achieve a Class 3 level which *"AS/NZS 2890.1 Off Street Parking Table 1.1" States as "Short-term and town centre parking, parking stations, hospitals, medical centres" requiring "Minimum for single manoeuvre entry and exit"* and therefore requires the use of a B85 5.8m swept path under *"AS/NZS 2890.1 B3 Swept Paths B3.1 (b)" which states "The B85 template comprises an inner pair of unbroken lines representing the B85 base dimension swept path, and an outer pair of broken lines which includes the manoeuvring clearance"* and *"AS/NZS 2890.1 B3 Swept Paths B3.2 (a)" states "a clearance of 300mm shall be added to both sides of the turning path"*. "Drawing 002 Rev2" details both this 5.8m swept path and the additional 300mm manoeuvring clearance. Further to this *"AS/NZS 2890.1 B4 Parking spaces and parking aisles B4.1" states "where there is an obstruction adjacent to a space, e.g. a wall, column, shrubs or landscaped area, an additional 300mm clearance is to be provided to the obstruction to achieve the desired level of service."* As this is the case adjacent to these spaces there is an additional 450mm clearance on both sides 1.5x the required amount.

Taking into account all the above documentation the manoeuvring clearance meets not only the minimal requirements but meets the most stringent of these as well.

There will be no gate between Right Of Way and parking spaces therefore no gate is shown on plans.

