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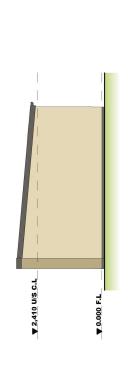
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PROPOSED VISITOR ACCOMODATION AT 28 OLIVE STREET, NEWSTEAD FOR T.D.GUNN Floor Plan REV. 2 DRAWING No. 003 PROJECT No. TG 001 DATE: 28/04/2021 DATE: 28/04/2021 SCALE: STATUS: CONVERSION DRAWN: T.D.GUNN AUTH: T.D.GUNN 1:50 Floor Plan Document Set ID: 4563019 Version: 2, Version Date: 24/05/2021

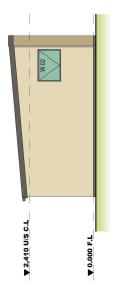




w.06 → W.04 → ▼ 2,410 U/S C.L ▼ 0.000 F.L

Eastern Elevation

Southern Elevation

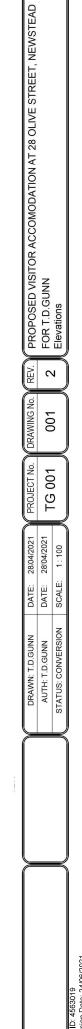


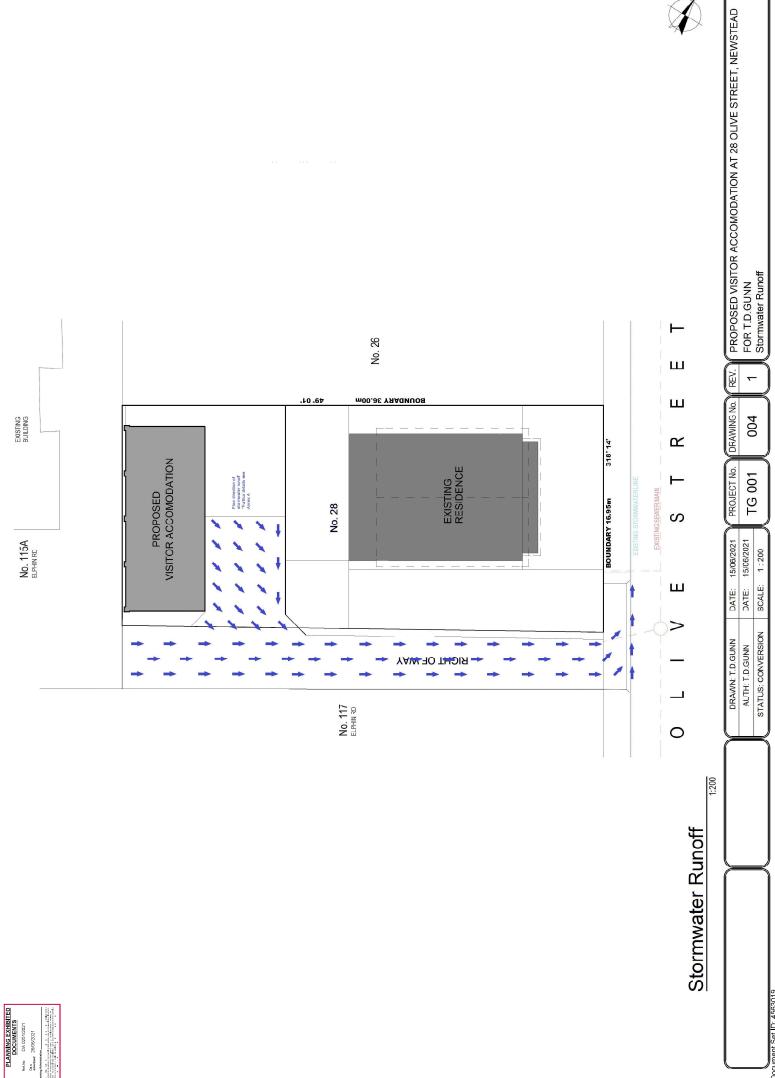
▼ 2,410 U/S C.L

Western Elevation

Northern Elevation

▼ 0.000 F.L





PLANNING EXHIBITED DOCUMENTS

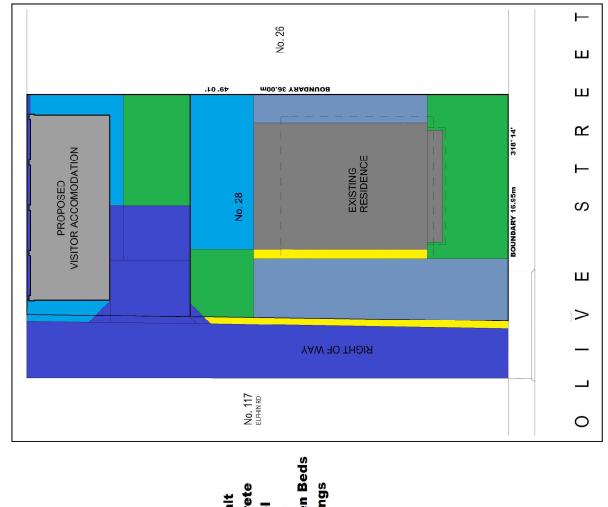
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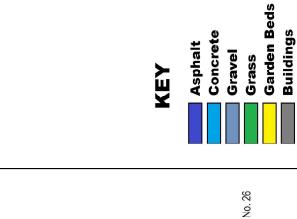
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PROPOSED VISITOR ACCOMODATION

**Proposed Surfaces** 





46، 01،

No. 28

No. 117 ELPHIN RD

**BOUNDARY 36.00m** 

EXISTING RESIDENCE

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318° 14'

BOUNDARY 16.95m

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	DRAWING N	900		
	PROJECT No.	TG 001		
	15/06/2021	15/06/2021	1:200	
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	DRAWN: T.D.GUNN	AUTH: T.D.GUNN	STATUS: CONVERSION SCAL	
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# PLANNING EXHIBITED DOCUMENTS

Ref. No: DA 0251/2021

Da e advertised: 26/06/2021

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## ANNEX A to DA0251/2021

### Parking Space Surface Treatment.

"E6.0 Parking and Sustainable Transport Code E6.6.1 A1 (b)" states "All parking, access ways, manoeuvring and circulation spaces must be formed and paved"

The finish to all these areas will be sealed asphalt as shown in submitted "Drawing 005 Rev1"

Earthworks required to achieve this will be the removal of 400mm of top soil in those areas where there is currently grass and replaced with 400mm of compacted road base prior to installation of asphalt surface. The existing areas marked as gravel in these location already consists of 400mm compacted road base.

## Parking space access.

"E6.0 Parking and Sustainable Transport Code Table E6.3" Calls for a combined access and manoeuvring width of only 6.4m to access a 5.4m x 2.6m, this proposal has over 7.5m

A higher standard exists in "AS/NZS 2890.1 Off Street Parking Table 1.1" which provides 6 different classifications of off street parking. As the property is located in the zoning for General Residential and is primarily a domestic property the most accurate class identification is Class 1A described as "Residential, domestic and employee parking" the requirement under this table is "three-point turn entry and exit into 90deg spaces only" and subsequently under "AS/NZS 2890.1 Off Street Parking Figure 2.2" require an aisle width or only 5.8m. Therefore there is no requirement for a B85 single manoeuvre 5.4m radius in using this class.

However there is adequate manoeuvring clearance to achieve a Class 3 level which "AS/NZS 2890.1 Off Street Parking Table 1.1" States as "Short-term and town centre parking, parking stations, hospitals, medical centres" requiring "Minimum for single manoeuvre entry and exit" and therefore requires the use of a B85 5.8m swept path under "AS/NZS 2890.1 B3 Swept Paths B3.1 (b)" which states "The B85 template comprises an inner pair of unbroken lines representing the B85 base dimension swept path, and an outer pair of broken lines which includes the manoeuvring clearance" and "AS/NZS 2890.1 B3 Swept Paths B3.2 (a)" states "a clearance of 300mm shall be added to both sides of the turning path". "Drawing 002 Rev2" details both this 5.8m swept path and the additional 300mm manoeuvring clearance. Further to this "AS/NZS 2890.1 B4 Parking spaces and parking aisles B4.1" states "where there is an obstruction adjacent to a space, e.g. a wall, column, shrubs or landscaped area, an additional 300mm clearance is to be provided to the obstruction to achieve the desired level of service." As this is the case adjacent to these spaces there is an additional 450mm clearance on both sides 1.5x the required amount.

Taking into account all the above documentation the manoeuvring clearance meets not only the minimal requirements but meets the most stringent of these as well.

There will be no gate between Right Of Way and parking spaces therefore no gate is shown on plans.

## PLANNING EXHIBITED DOCUMENTS

Ref. No: DA 0251/2021

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### Parking space dimensions.

"AS/NZS 2890.1 B4 Parking spaces and parking aisles Table B1" shows class 1 & 1A at 2.4m wide and class 2, 3 & 3A at 2.6m wide, the previously stated additional 300mm clearance from walls is in addition to these dimensions.

Secondarily to the above standard, "E6.0 Parking and Sustainable Transport Code Table E6.3" Calls for a 5.4m x 2.6m based on the proposed manoeuvring width and 2.6m being the higher of the two standards is as such is shown in submitted "Drawing 002 Rev2"

Again taking into account all the above documentation the proposed parking space dimensions meet all standards with excess additional clearance space then required.

#### Stormwater runoff and parking space gradients.

"E6.0 Parking and Sustainable Transport Code E6.6.1 A1 (a) & (b)" states "All parking, access ways, manoeuvring and circulation spaces must have a gradient of 10% or less & be drained to the public stormwater system, or contained on the site"

For maximum gradients "AS/NZS 2890.1 Off Street Car Parking 2.4.6 Gradients within parking modules 2.4.6.1 Maximum gradients (a) & (b)" states "Measured parallel to the angle of parking – 1 in 20 (5%)" & "Measured in any other direction – 1 in 16 (6.25%)"

For minimum gradients "AS/NZS 2890.1 Off Street Car Parking 2.4.6 Gradients within parking modules 2.4.6.2 Minimum gradients" states "so that parking floors will drain adequately, the minimum gradient shall be 1 in 100 (1%) for outdoor areas."

The gradient from South West to North East of the parking spaces (parallel to the angle of parking) will be 5% the gradient and from North West to South East of the parking spaces will be 5.9% this will result in stormwater draining from the parking spaces in an easterly direction until it reaches the right of way and then continue North East down the Right Of Way (approx. 4% Gradient) until making its way to the storm water drains as shown in submitted "Drawing 004 Rev1"

These gradients therefore meet the above stated requirements.